



Urban Design Brief

655 Eliza Street, Arthur, Wellington North
February, 2025



TABLE OF CONTENTS

01 INTRODUCTION	4
INTRODUCTION	5
DOCUMENT STRUCTURE	5
02 PLANNING CONTEXT & SITE ANALYSIS	6
STUDY AREA DESCRIPTION	7
STUDY AREA INVENTORY	10
PLANNING POLICY ENFORCEMENT	14
03 THE PROPOSAL	20
DESIGN PRINCIPLES	21
LAND USE	31
SITE DESIGN	33
CONCLUSION	36

1 INTRODUCTION

1.1. INTRODUCTION

This Urban Design Brief has been prepared in support of a proposed development on the lands municipally known as 665 Eliza Street, and a vacant property on the west side of Eliza Street (the “Subject Site”) for residential purposes. The proposal includes the development of a residential subdivision consisting of 815-866 single-, semi-detached, and townhome typologies, as well as public park space, public roads, and stormwater management features. The Subject Site is legally described as:

PT LT 1 CON 2 WEST LUTHER AS IN
RON74408; WELLINGTON NORTH
PART PARK LOTS 1 AND 2 NORTH OF
MACAULEY STREET SURVEY CROWN
ARTHUR VILLAGE; PART 2, 61R22310
TOWNSHIP OF WELLINGTON NORTH

This Urban Design Brief outlines best practices for the design of the public, private and semi-private realm for the Subject Site within the context of the Town of Arthur, the Township of Wellington North, and Wellington County. The goal of this document is to ensure excellence in urban design of the proposed residential development while addressing the character of the neighbourhood and promoting increased density, walkability, and sustainability. The guiding design principle is to provide a diversity of housing types and a balance between the natural and built environment.

The proposed development will accommodate a range of architectural elements consistent with the existing and emerging neighbourhood identity. The proposal will enhance the character and image of the established area, prioritizing pedestrian safety and sense of place, reinforcing a human-scaled built form, and promoting social interaction in the urban environment through architectural, urban design and landscape strategies. Through text supported by photographs, diagrams and illustrations, this document describes how the proposed site design and urban design forms evolved to demonstrate compliance with the Municipality’s policies and guidelines.

1.2. DOCUMENT STRUCTURE

This document is organized into three sections:

- Section 1 - The Policy Context and Site Analysis (policies, location and community context);
- Section 2 - Vision and Design Principles;
- Section 3 - The Design Concept that is responsive to the policy context and site analysis

2

POLICY CONTEXT & SITE ANALYSIS



2.1. Study Area Description

2.1.1 Site Location

The Subject Site (Figure 1) is located at the north end of the community of Arthur. The Subject Site is approximately 57 hectares in size and currently accommodates agricultural uses. Eliza Street bisects the Subject Site and 2nd Line is situated directly north. There is a single residential dwelling and barn structures at 665 Eliza Street. The Subject Site is situated approximately 1km north of Arthur's town centre. Surrounding land uses are further described on the following page.



Figure 1. Subject Site

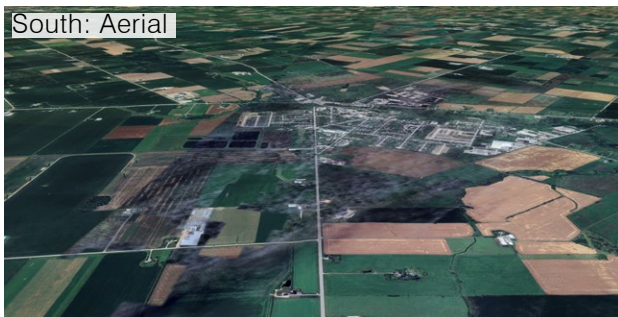
2.1.2 Surrounding Context

The Subject Site is surrounded by agricultural and rural commercial uses. (Figure 2). This includes Ivan Armstrong Trucking to the northwest, Wilders Kennel & Dog Training to the northeast, and Clark Brothers Contracting to the south. The parcel south of the Subject Site is zoned as Industrial (M1-2), parcels to the east are zoned Agricultural Wind (AW-1) and the property to the north is Agricultural (A-1).

A detailed summary of the uses surrounding the Subject Site is included on the following pages (Figure 3 and 4):



Figure 2. Surrounding Context



NORTH

North of the Subject Site are agricultural lands, a dog kennel and training centre, rural residential dwellings and barn structures associated with agricultural lands, and a mechanic warehouse.

EAST

East of the Subject Site is characterized by agricultural lands.

SOUTH

Immediately south of the Subject Site are lands used for Agricultural purposes within the urban boundary of Arthur, and a general contracting workshop. Further south along the east side of Eliza Street are the Town's sewage lagoons.

WEST

West of the Subject Site are agricultural lands and the unopened Wells Street.

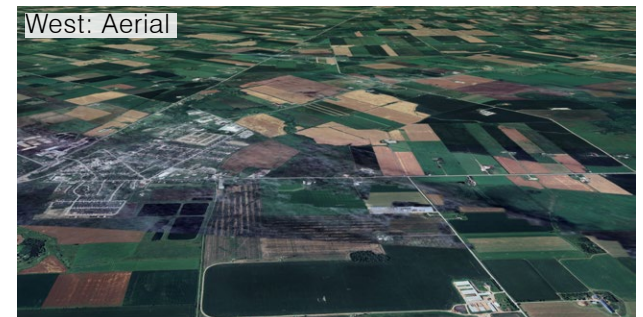
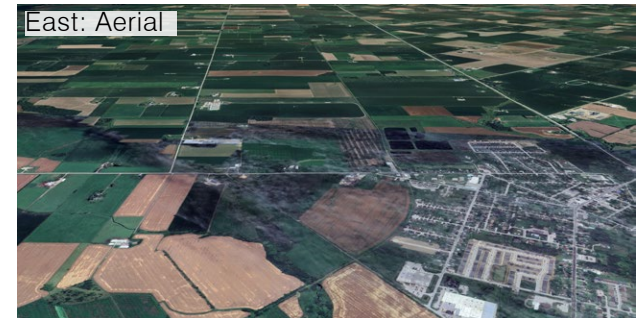


Figure 3. Surrounding Context: North and South (Google Earth)

Figure 4. Surrounding Context: East and West (Google Earth)

2.2. Study Area Inventory

PUBLIC SERVICE FACILITIES

The Subject Site is within a 4-kilometre radius of several key public service facilities located in Arthur (Figure 5). These facilities are entirely within the settlement area boundary of Arthur. A breakdown of the public service facilities available to the Subject Site is detailed below:

- Wellington County Library Arthur Branch, located approximately 1.5 kilometres south of the subject lands on Charles Street E.
- Schools in the community of Arthur include Arthur Public School, St. John Catholic School and Arthur Christian School, with St. John Catholic School being the closest to the subject lands at approximately 750 metres south.
- Behind St. John Catholic School, south of the intersection of Tucker Street and Eliza Street, there is a sports facility complex that includes a hockey arena, a curling rink, a swimming pool, a children's park and several baseball diamonds.
- The Subject Site is serviced by fire emergency services, with Arthur Fire Station located on Highway 6 near Frederick Street E. The closest Ontario Provincial Police station and hospital are located in the community of Fergus, approximately 23 kilometres southeast.

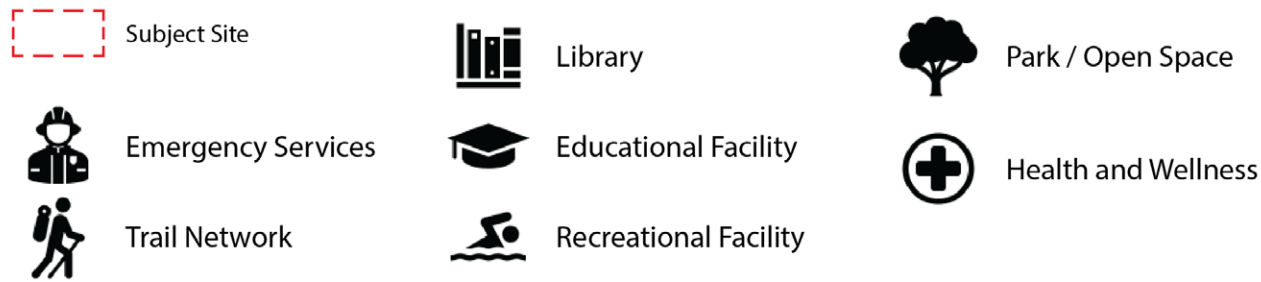
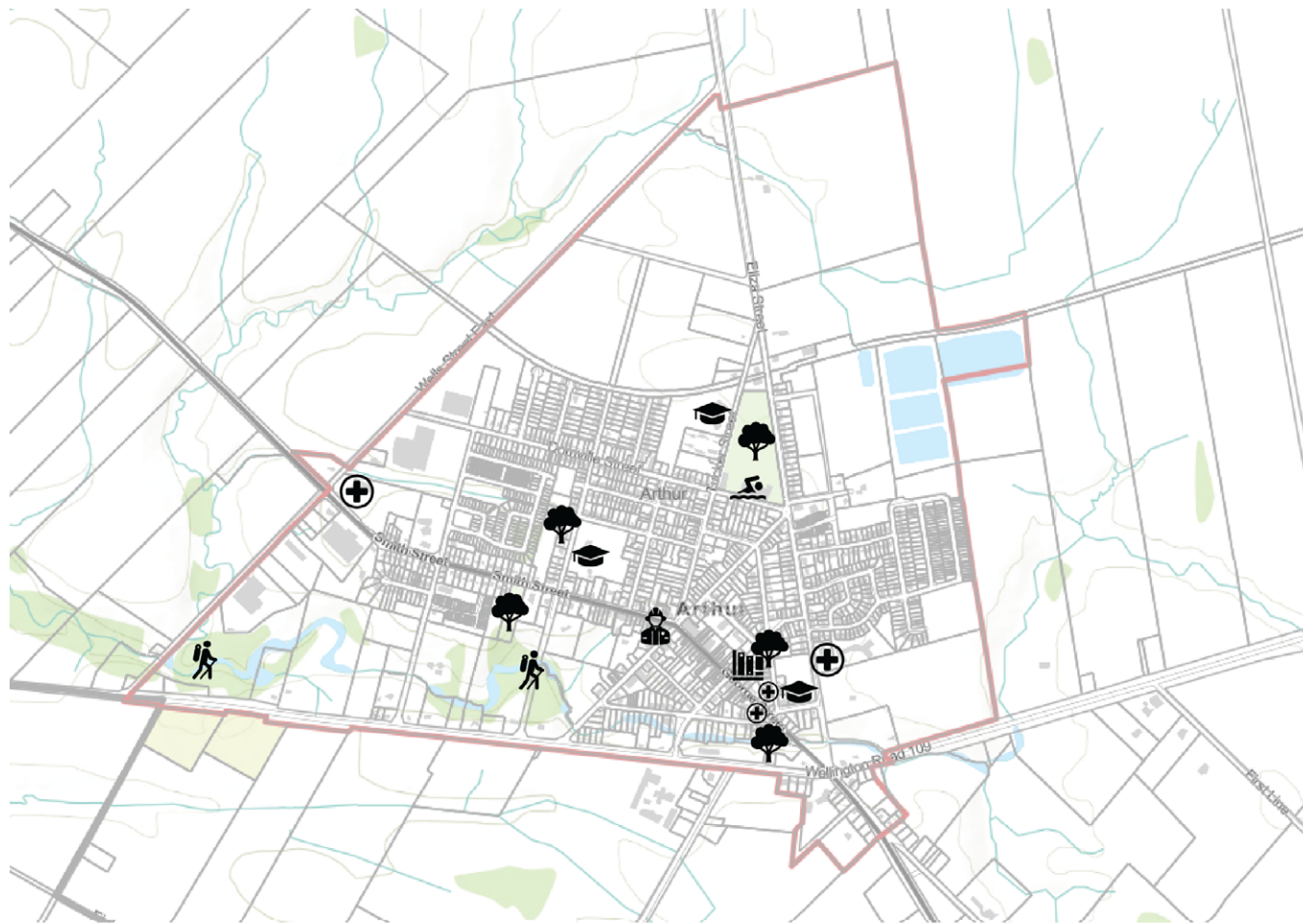


FIGURE 5: Public Service Facilities

TRANSPORTATION NETWORK

- The Subject Site fronts onto Eliza Street and Wells Road E. with nearby access to Provincial Highway 6 (Smith Street) to the south and west. Eliza Street (Wellington Road 14) is classified as a Country Road while Wells Street E is a local road (figure 7).
- There are minimal existing active transportation options for accessing the Subject Site and within the Town of Arthur itself. Neither Eliza Street nor Wells Street host sidewalks or bike lanes, but Eliza Street does have a paved shoulder. The proposed development will incorporate sidewalks throughout it.
- The Arthur Hiking River Trail runs for approximately 2.8km east-west along the southern edge of Arthur. There is a connection to Highway 6 (Smith Street) at the start/end of the Arthur Hiking River Trail.
- There is no transit service to the town of Arthur, nor the Municipality of Wellington North. A variety of local and regional transit services exist to the south in the City of Guelph; including GO Bus routes to Mississauga, Toronto, Highway 407 Terminal and Waterloo/Hamilton. Guelph also hosts local transit (Guelph Transit) which connects users to a variety of destinations in the City. Closer to the Community of Arthur is the Town of Orangeville, which also hosts a local transit network (Orangeville Transit) and GO Bus route 37 (Brampton).

2.3. Planning Policy Enforcement

This Urban Design Brief includes a summary on the planning framework of provincial, regional, and municipal policy documents, and guidelines that provide the basis for development in the Wellington North community. The section below outlines the overarching goals that provide the direction for more sustainable communities. Each policy section summary comprises key points extracted from it to help inform the decision making for the proposed development.

The proposed development is consistent with the following policies:

- Provincial Policy Statement (2024);
- County of Wellington Official Plan (2024); and
- Township of Wellington North Zoning By-law 66-01 (March, 2023 Consolidation).

2.3.1 Provincial Planning Statement (2024)

The Provincial Planning Statement (PPS), 2024 is a province-wide land use planning policy framework. It is a policy statement issued under the authority of Section 3 of the Planning Act and provides policy direction on matters of provincial interest related to land use planning and development as of October 20, 2024. Accordingly, decisions affecting planning matters shall be consistent with the PPS 2024.

Policy

Policy 2.3.1.2. The PPS states that land use patterns within settlement areas should be based on densities and a mix of land uses which efficiently use land and resources, optimize existing and planned infrastructure and public service facilities, support active transportation;

Policy 2.2.1.b - The PPS states that planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by: permitting and facilitating all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents as well as permitting all types of residential intensification, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units.

Response

The proposed residential development is consistent with the PPS's intensification and mixed land use policies.

The proposed residential development will:

- Accommodate an appropriate range and mix of residential and commercial units;
- Aid in increasing the current mix of residential uses in the area surrounding the Subject Site to promote efficient development and land use patterns;
- Promote intensification in the municipality and will optimise the use of existing infrastructure and transit investments;
- Minimize land consumption and climate change impacts;
- Provide for an appropriate mix and range of residential typologies to meet long-term needs of future and existing residents; and,
- Encourage more compact residential uses to allow for the efficient use of land, promote growth and development, and avoids risks to public health and safety.

2.3.2 County of Wellington Official Plan (2024)

The County of Wellington Official Plan (WCOP) contains policies which pertain to both the County as a whole (“Regional Policies”), and more locally for the constituent Municipalities and townships therein (“Local Policies”). The Township of Wellington North does not have its own Official Plan.

Policy

The Subject Site is designated as Future Development in Schedule B6-2 (Figure 8).

Section 8.10 of the WCOP pertains to Future Development. It has been deemed not possible to pre-designate Future Development lands for the future intended uses. The permitted uses within areas designated as Future Development shall be limited to existing uses and the growing of crops including nursery and horticultural crops but does not include greenhouses.

Response

The proposed OPA seeks to redesignate the lands from *Future Development* to *Residential* to permit the development of single-detached, semi-detached and townhome dwellings.

The proposed residential development conforms to the *Residential* Designation by:

- ensuring an adequate supply of land is available for anticipate population growth;
- providing a variety of dwelling types to satisfy a range of residential requirements;
- encouraging intensification, development proposals provided they maintain the stability and character of existing neighbourhoods; and
- encouraging residential developments which incorporate innovative and appropriate design principles which contribute to public safety, affordability, energy conservation and that protect, enhance and properly manage the natural environment.

The proposed development is comprised of low and medium density development which are permitted uses within the *Residential* designation.

2.3.3 Wellington North Zoning By-Law 66-01

The Township of Wellington North Zoning By-Law (ZBL) 66-01 states what type of land uses are permitted in a specific area or zone and provides the requirements or regulations that apply to such land.

Policy

The Subject Site is zoned as Future Development (FD) on Schedule A Map 2 Arthur Detail of the ZBL (Figure 10). Section 29 of the ZBL covers Future Development Zones.

Permitted uses within the Future Development Zone:

- Uses, buildings and structures lawfully existing on the date of passing this By-law.

Response

The proposed Zoning By-law Amendment seeks to rezone the lands from *Future Development* to R2-XX *Residential* to permit the development of single-detached, semi-detached and townhome dwellings.

Section 12 of the ZBL pertains to R2 Residential. The following uses are permitted in the R2 Zone:

- Single detached residential dwelling;
- Semi-detached residential dwelling;
- Duplex dwelling;
- Triplex residential dwelling;
- Fourplex residential dwelling;
- Three or Four Unit Street Townhouse;
- An accessory Bed and Breakfast establishment (Class 2);
- Boarding, lodging or rooming establishment not exceeding 5 rooms or dwelling units for let;
- Additional Dwelling Unit in accordance with Section 6.29;
- Home Occupation and
- Accessory uses buildings and structures

R2-XX, as enclosed within this application package, provides site specific performance standards to enable flexibility in design and built form of proposed dwellings on the Subject Site.

The proposed development is comprised of low and medium density development which are permitted uses within the R2 *Residential* designation.

Section 12.3 Landscaped Area indicates:

1. No part of any required front yard or required exterior side yard associated with a single detached dwelling and semi-detached dwelling or a duplex dwelling shall be used for any purpose other than a landscaped area except for a driveway leading to a private garage.
2. A minimum of 45% of the required front yard and required exterior side yard shall be maintained as landscaped area as proposed via the R2-XX.

The proposed development shall conform to the requirements outlined in the Wellington North ZBL 66-01 for R2 Residential zones, with site specific performance standards.

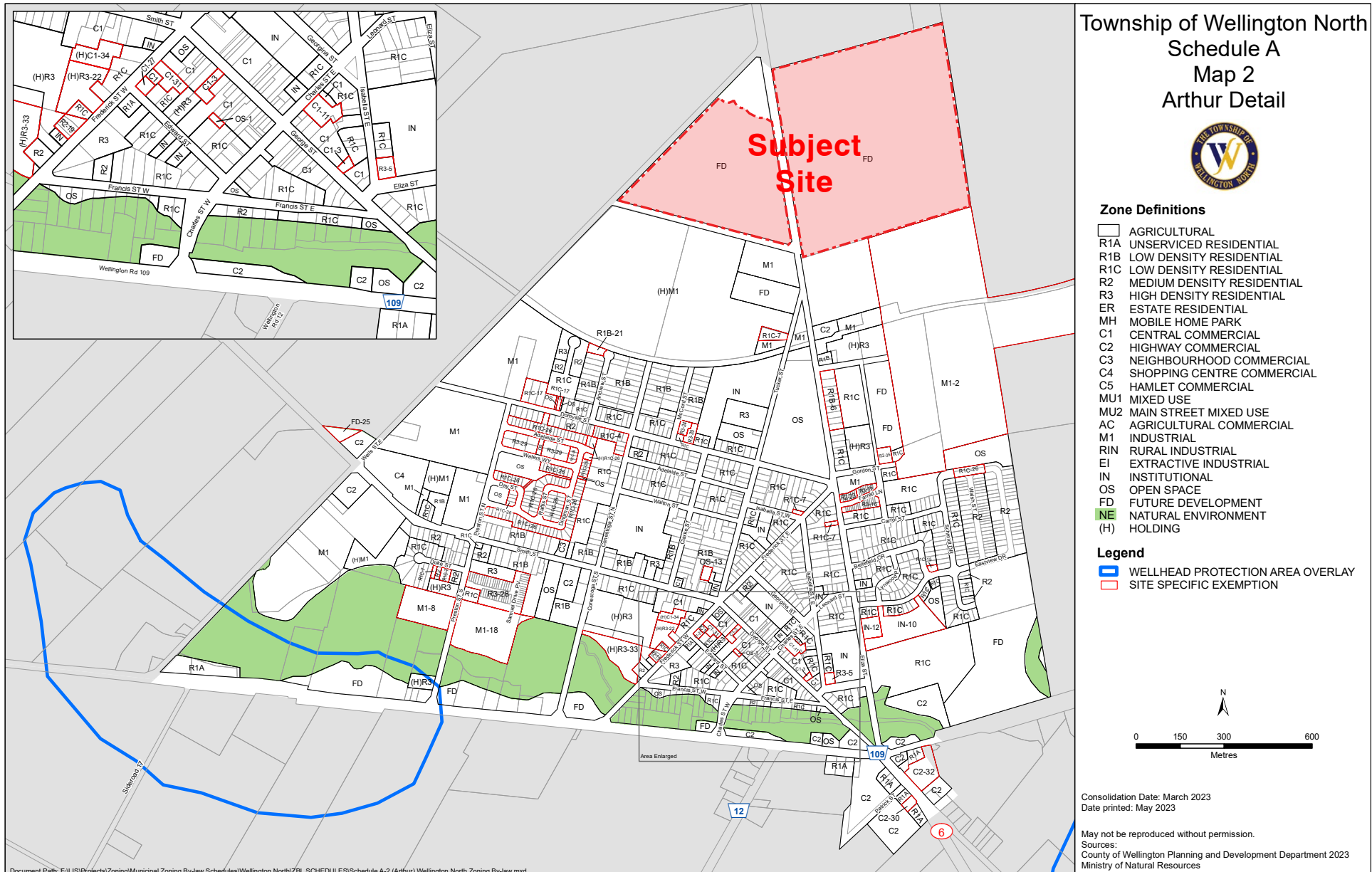


FIGURE 8: Wellington North Zoning By-Law 66-01 - Schedule A Map 2 Arthur Detail

3 THE PROPOSAL

3.1. DESIGN PRINCIPLES

The urban design vision for the proposed development is to create pedestrian-friendly streetscapes through innovative and sustainable building and landscape designs. Contemporary building solutions will combine with traditional architectural styles to maintain the small-town character and image of the community while contributing to a more sustainable future.

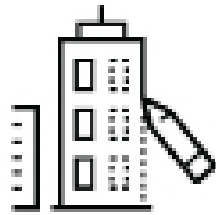
The following guidelines provide an outline of design initiatives specific to informing the proposal and design of development for the Subject Site. The supporting urban design principles have been summarized and outlined below for the Subject Site proposal.

The urban design goals and vision are to promote increased density, walkability, encourage cycling, and accommodate a range of architectural elements consistent with the existing small-town character of the neighbourhood. The guiding design principle is to draw on the balance between existing and new, low- and medium-density dwellings.

The detailed design principles are analyzed as per the following:



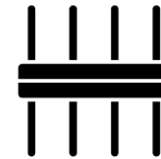
CHARACTER & IMAGE



BUILDING DESIGN



GARAGES



PRIORITY LOTS



**COMMUNITY GATEWAY
AND WINDOW**



CHARACTER AND IMAGE

The proposed development is intended to complement and support the existing character of the community and to create a comfortable people-scale urban environment. Character and Image will be implemented through the following actions:

PEDESTRIAN SCALE.

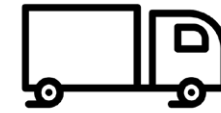
- Pedestrian comfort is a priority for the site and is to be promoted through architectural elements brought closer to the street through the use of minimum setbacks of 6.0 metres for front yards and 7.5 metres for rear yards.
- Scale is of importance for the proposed development. Building setbacks will promote an appropriate pedestrian comfort at street level as will the maximum permitted building height.
- As previously indicated, sidewalks are proposed on both sides of the road throughout the proposed development.

DENSITY

- The semi-detached and street-town units will provide increased density and a strong streetwall.

NATURAL HERITAGE

- A relationship between the proposed built form and natural environments are extremely important for the surrounding site. A Natural Heritage Feature runs across the site from the northwest down to the southeast of the Subject Site.



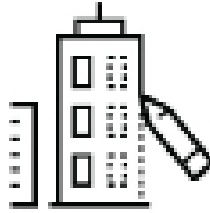
GARAGES

The proposed development situates garages in ways to minimize their impact at the street level as well as proposed buildings. Appropriate Garage design will be accomplished through the following.

TOWNHOUSE GARAGES.

Townhouses will have single-car garages facing the public roadway. Attached garages will not dominate the massing of the street-facing dwelling facade. In general, the massing of the garage should be minimized by:

- Giving the habitable portion of the dwelling a larger and more dominant mass.
- Integrating the massing of the garage into the main massing of the dwelling.
- Positioning the main front wall and porch face closer to the street by way of setback requirements.
- Garage entrances are recessed 1.5 metres from the front facade of the building to limit the impact of the garage on the front elevation of the buildings.
- Design quality, colour, and material will be complementary to the main dwelling as indicated on the Elevations.
- Garages shall be paired for the townhouse units when possible.
- Driveways will not occupy more than 55% of required front yard.



BUILDING DESIGN

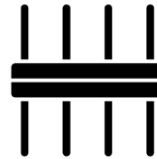
The proposed development includes approximately 815 to 866 total units comprised of approximately 454-504 Single Detached units, 112-113 Semi-Detached units and 249 Street Townhouse units. Corner lots will have an upgraded façade on the flanking wall to promote an inviting streetscape. In all instances, buildings shall display the following general building design criteria:

DESIGN AND MASSING.

- Building architecture and landscaping should define the public and private boundaries in a clear way.
- Stairs accessing the main entrance to the dwelling are designed as an integral component of the dwelling's facade.
- Appropriate design variety has been incorporated throughout the project. Distinct articulation in both the front and rear elevations, provide variation between buildings. Key elements such as front entries and façade detailing have been carefully considered to ensure each unit has its own identity.
- Three different color palettes for the façade elevations have been selected to avoid monotony, further enhancing the visual diversity.
- Buildings frontage width will vary. The following frontages will be provided:
 - Single Detached frontages: 30'
 - Single Detached frontages: 36'
 - Single Detached frontages: 40'
 - Freehold Townhome frontages: 20'
 - Semi-detached frontages: 25'
- Appropriate design variety between adjacent blocks will be provided as indicated in Figures 10, 11, 12, 13 and 14.

EYES ON THE STREET

- Ample windows face public areas including the parkette following CPTED (Crime Prevention Through Environmental Design) principles.
- Front porches, balconies, recessed entries, verandahs and other types of transitional spaces should allow for views along the street.
- The main front entry are be clearly identifiable for each townhouse unit through the use of porches, stairs, facade articulation and address numbers.



PRIORITY LOTS

Dwellings in prominent locations, or “Priority Lots”, should be designed to reflect their high-profile location (Figure 11). At these locations, building components and design should have a higher degree of visibility within the public realm that emphasize their potential role as orienting structures and landmarks within the neighbourhood.

VIEW TERMINUS.

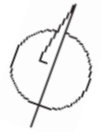
View Terminus Dwellings play an important role in the streetscape by terminating a long view corridor. Corner lots opposite these dwellings should frame the view from the street. Because of their prominence within the subdivision, View Terminus Dwellings should include enhancement features such as:

- Front entrances align with the view terminus as much as possible rather than double driveways.
- Driveways are kept on the outside of View Terminus Dwellings as much as possible.
- Architectural treatments that provide greater visual interest for the passing public will be incorporated including distinct articulation in both the front and rear elevations, providing variation between the townhouse buildings.





CORNER LOT.

Corner Lot Dwellings shall address both street frontages in a consistent manner and incorporate ground level detailing (porches, windows, entries, etc.) that reinforces the pedestrian scale of the street. Buildings should be sited close to the street to help define the street edge. The following architectural elements are to be included:

- Both street frontages are addressed in a consistent manner.
- Bay windows have been incorporated at corner lots to address the street and provide visual interest rather than blank walls.
- Buildings will be sited close to the street.
- The main entrances face the front lot line provided appropriate attention is paid to the design of the flanking wall through the use of bay windows and/or a secondary entrance.



LEGEND

-  GATEWAY DWELLINGS
-  VIEW TERMINUS LOTS
-  UPGRADED REAR & SIDE ARCHITECTURE
-  CORNER LOTS

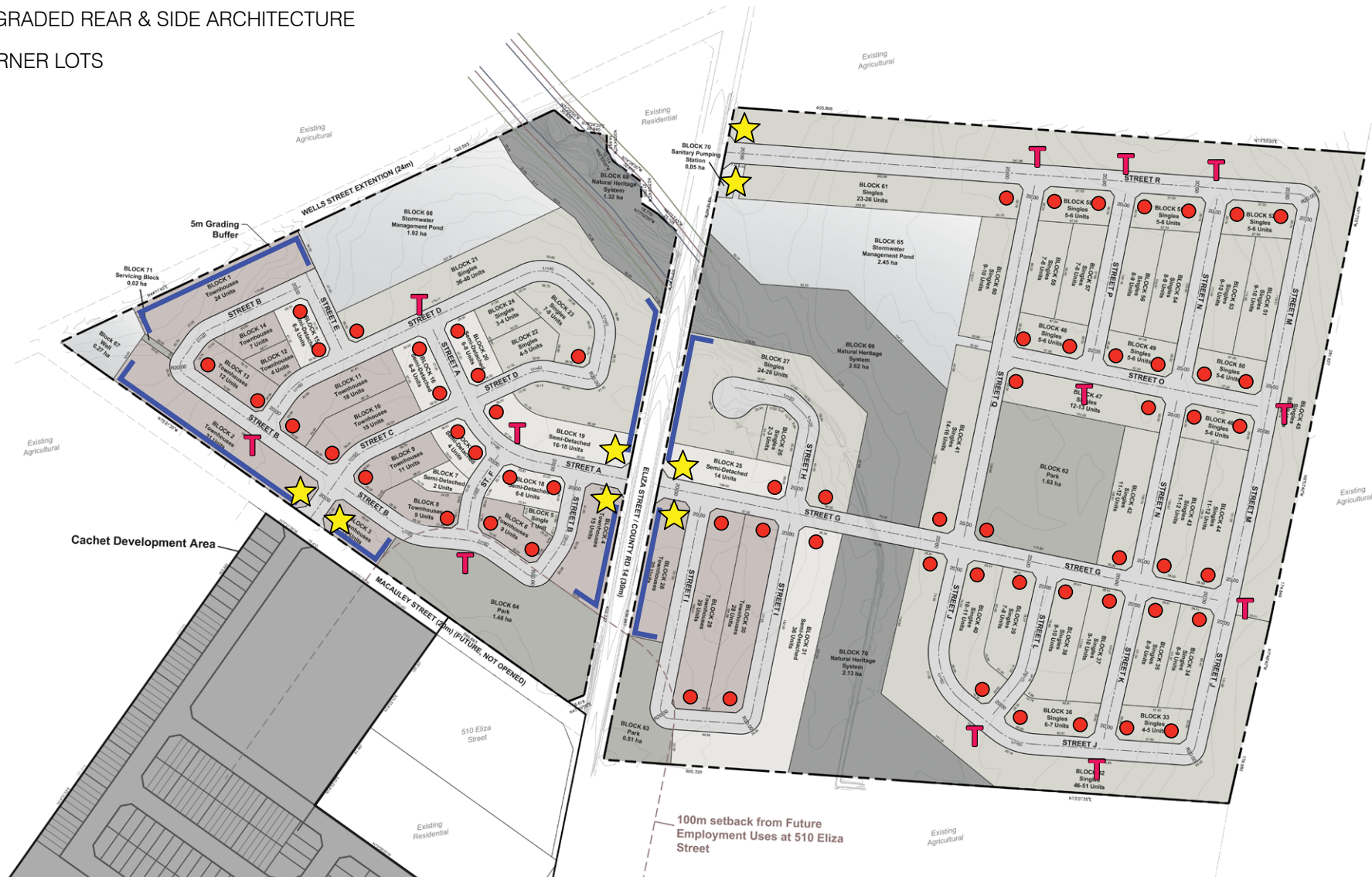
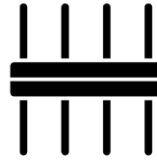


FIGURE 9: Priority Lots



PRIORITY LOTS

GATEWAY DWELLINGS

Community Gateway Dwellings are proposed along the entrances to the Subject Site at Eliza Street and Macauley Street. Community Gateway Dwellings play an important role in expressing the image, character and quality of the community to residents, visitors and passersby. A high degree of architectural design quality will be expected for all elevations of Community Gateway Dwellings.

- The main entrance should be oriented to the higher order street or to the daylight triangle.
- Distinctive architectural elements such as turrets, tower features, projecting bays, masonry chimneys, precast detailing, shutters and gables shall be employed where architecturally appropriate.
- Special attention to the exterior colour package is required to encourage variety throughout the site and to help identify gateway buildings.
- A wraparound porch or other similarly dominant design feature shall be provided.
- Porches are sufficiently setback from any community gateway entry feature to avoid conflicts.

UPGRADED REAR AND SIDE ARCHITECTURE.

Side architecture is required where these elevations are exposed to public view.

The proposed development incorporates features such as:

- Enhanced window style, muntin bars, shutters, frieze board, quoining/pilasters, decorative panels/louvres and brick detailing consistent with the front elevation.
- The introduction of gables, dormers and/or bay windows.
- Additional fenestration on the exposed side elevation.
- Some variety among rear yard setbacks or rear wall articulation will be considered for lots having long stretches of high exposure rear elevations.
- The level of upgrading should be consistent with the level of public exposure.



Figure 10: Building Renderings - Sample 36' Frontage Detached Units



Figure 11: Building Renderings - Sample 40' Frontage Detached Units



Figure 12: Building Renderings - Sample 25' Wide Frontage Semi-detached Units



Figure 13: Building Renderings - Sample 20' Wide Frontage Townhouse Units



Figure 14: Building Renderings - Sample 20' Wide Frontage Townhouse Units with Rear Garages

3.2. Land Use

The proposed development includes approximately 815 to 866 total units offers an increase in housing options and provides an increase in density within the Community of Arthur. The approximately 454-504 Single Detached units, 112-113 Semi-Detached units and 249 Street Townhouse units will provide a more compact built form and a more efficient use of land.

The denser built form of Freehold Townhouses and Semi-Detached homes are concentrated in the southwest portion of the Subject Site, while the northeast is occupied primarily by 36' and 40' single family homes. This distribution of density reflects the surrounding built form, where the southwest portion of the Subject Site is adjacent to existing residential and commercial uses, while the northeast portion of the site is adjacent to existing farmland.

A 100m setback is provided from the existing commercial uses directly south of the Subject Site. Parkland is situated within this buffer and no residential development will occur within this buffer. Special architectural attention will be paid to residential units facing the commercial uses. An additional square park is located on Block 62. A total of 3.62 ha of park is provided in the proposed development. Natural Heritage Lands run from the northwest edge to the southeast of the edge of the Subject Site. Two spaces for Stormwater Management are reserved on the Subject Site as well as lands for Well a on Block 67.

The proposed buildings will be designed to complement and support the small-town feel of the Community of Arthur. Their height, massing, and architectural details will complement the existing neighbourhood as a whole.



LEGEND

- 36' AND 40' SINGLES
- 20' FREEHOLD TOWNHOUSES
- 25' SEMI-DETACHED
- PARK
- STORMWATER MANAGEMENT POND
- NATURAL HERITAGE SYSTEMS
- SETBACK FROM EMPLOYMENT USE

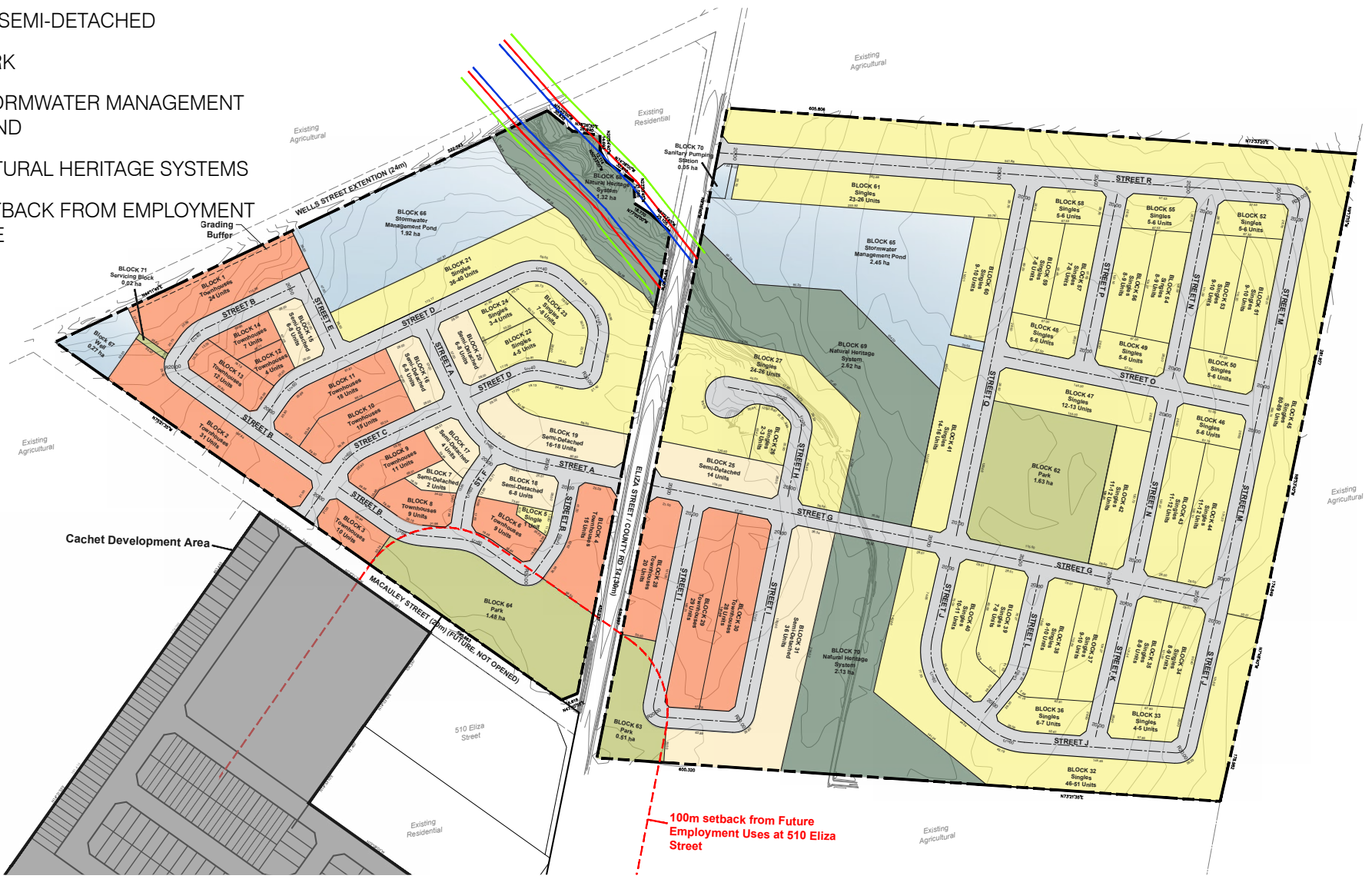


Figure 15: Land Use

3.3. Site Design

The Subject Site is an excellent candidate for residential intensification due to its inclusion within the Urban Centre Boundary of Arthur as well as its designation as Future Development in the WCOP. The proposed design of the buildings and their orientation is sensitive to the surrounding area and will maintain and celebrate the small-town feel of the Community of Arthur.

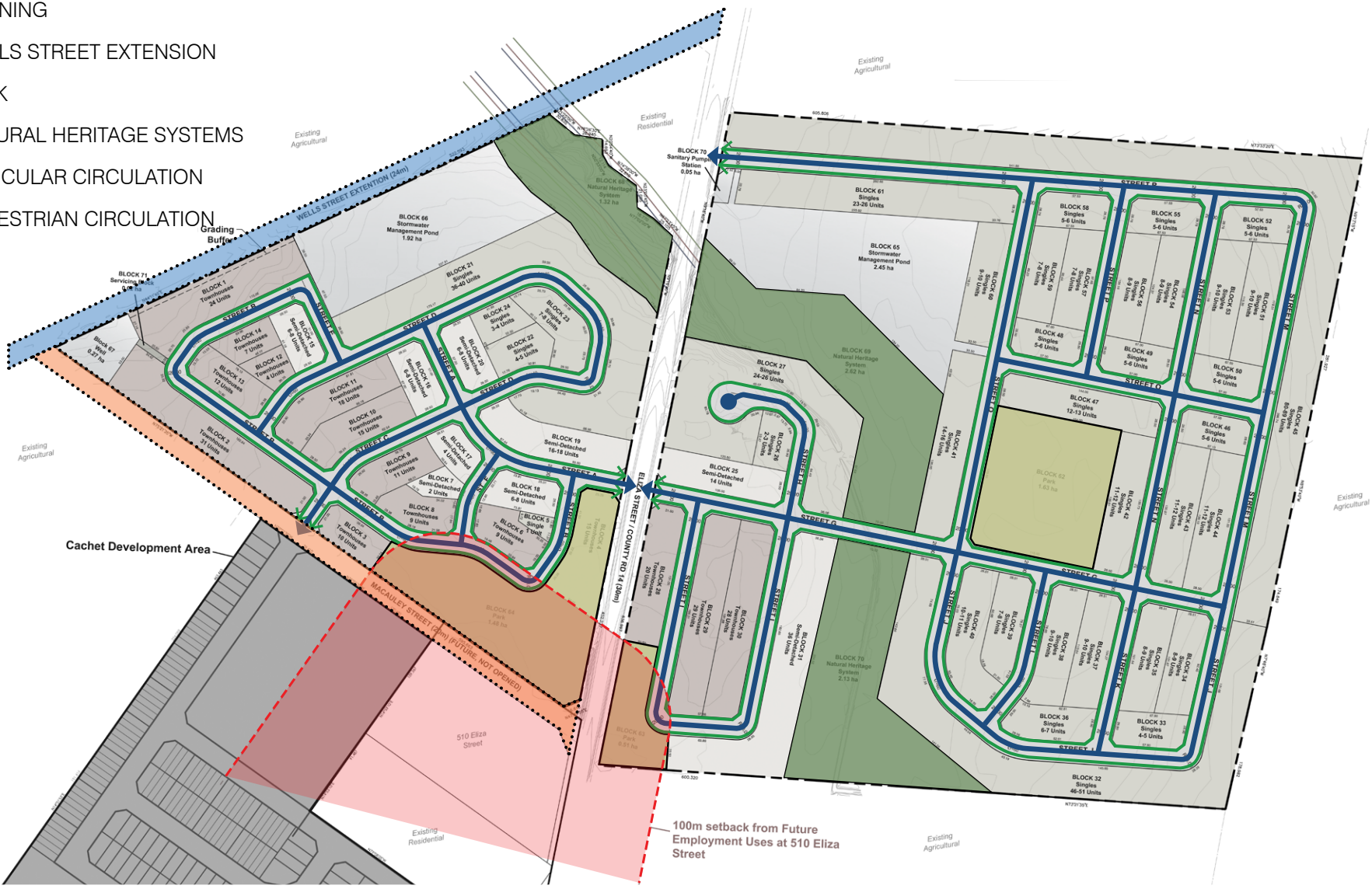
The existing Core Greenlands will be retained and protected. Two parks are situated at the south of the Subject Site with frontage along Eliza Street within the 100m Setback from existing Employment land use. A third park is situated in the middle of the eastern portion of the Subject Site. The central location of the parks will ensure green space is available for new residents and existing residents in the adjacent neighbourhoods.

Along the northwestern edge of the Subject Site Wells Street will be extended. Along the southwestern edge of the Subject Site Macauley Street will be opened.

Vehicular access will be provided from the new Macauley Street as well as Eliza Street. Along both sides of the newly proposed streets will be sidewalks to help encourage walkability and active transportation for the new neighbourhood. These new sidewalks will provide pedestrian connections to Eliza Street and Macauley Street.

LEGEND

- SETBACK FROM EMPLOYMENT USE
- FUTURE MACAULEY STREET OPENING
- WELLS STREET EXTENSION
- PARK
- NATURAL HERITAGE SYSTEMS
- VEHICULAR CIRCULATION
- PEDESTRIAN CIRCULATION



100m setback from Future Employment Uses at 510 Eliza Street

Figure 16: Site Design



CONCLUSION

The proposed development conforms with The County of Wellington Official Plan and the Township of Wellington North Zoning By-Law 66-01. Urban design and architectural strategies will be incorporated in order to promote a safe, pedestrian-friendly and comfortable built environment. The established small-town community character will be expanded through complementary building designs and landscape design strategies.